

IMPROVEMENT PLANS FOR HOOPER STREET (SOUTH) REHABILITATION IMPROVEMENTS PORT OF STOCKTON

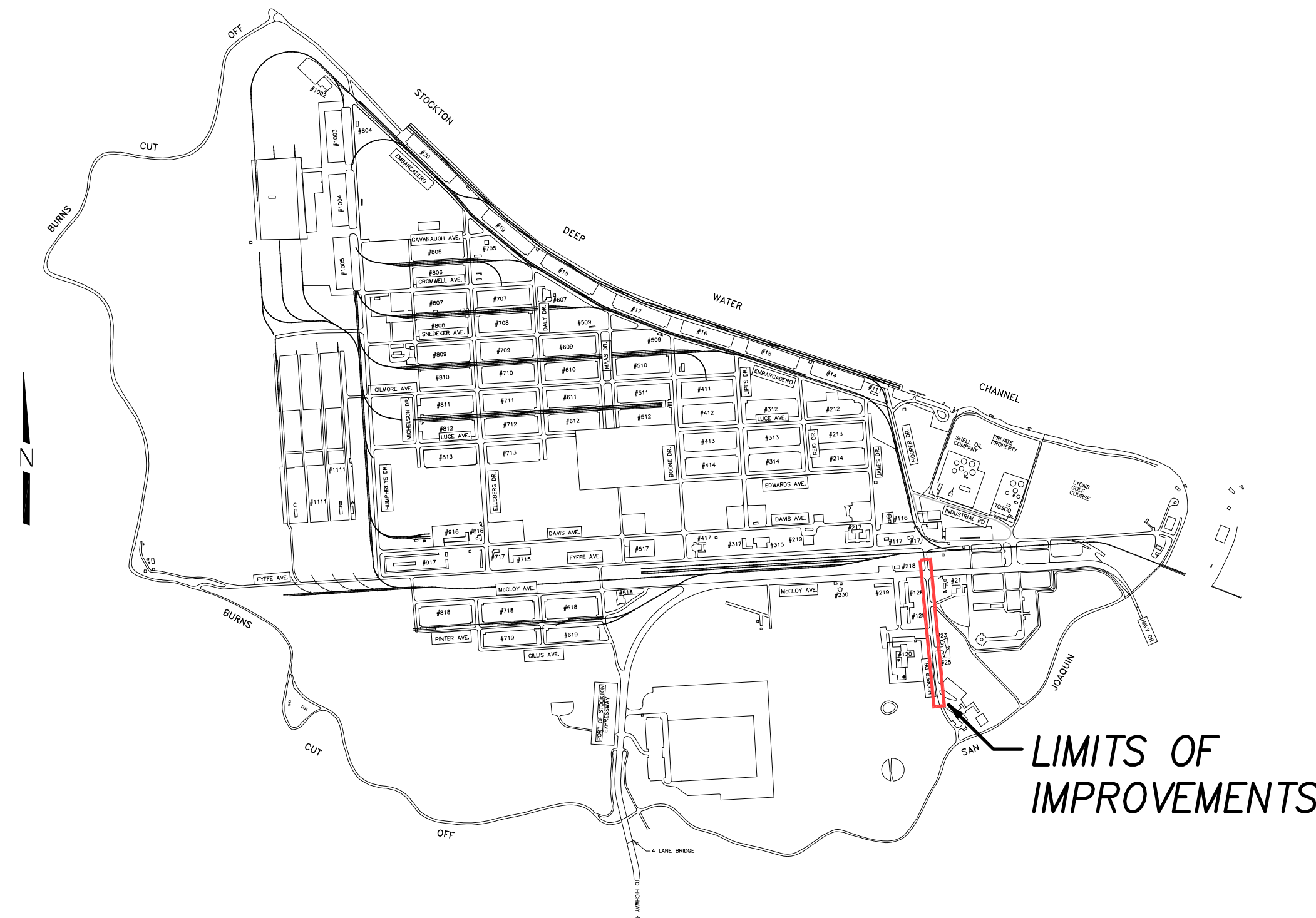
INDEX OF SHEETS

SHEET NO.

1. TITLE SHEET
2. BEGIN IMPROVEMENTS STA. 100+85 TO STA 111+39
3. STA. 111+39 TO 116+00 END IMPROVEMENTS & CROSS SECTION DETAILS
4. EROSION CONTROL PLAN & DETAILS

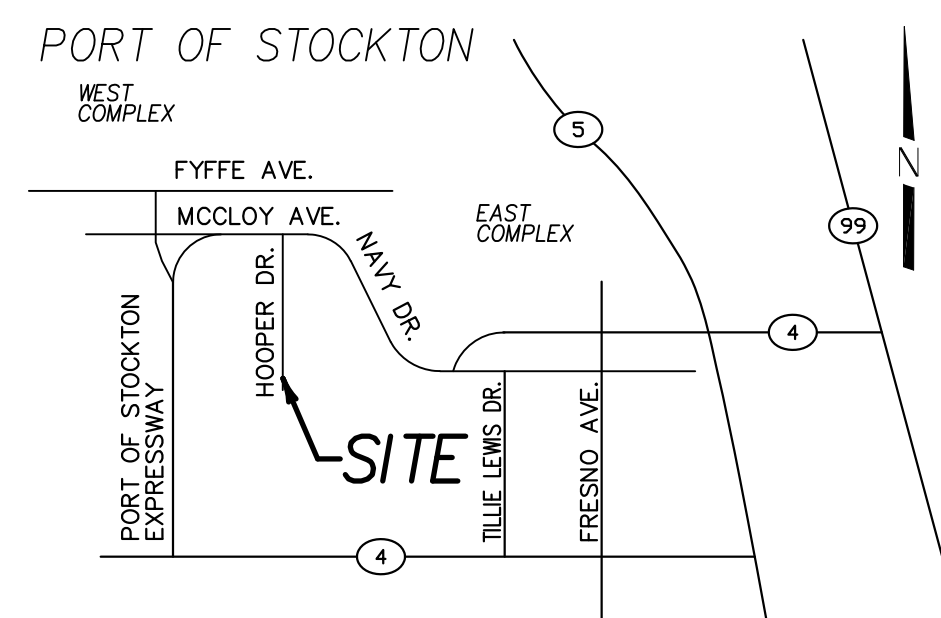
GENERAL NOTES

1. ALL WORK SHALL CONFORM TO THE CITY OF STOCKTON, CALTRANS, AND UNION PACIFIC RAILROAD CONSTRUCTION SPECIFICATIONS AND STANDARD PLANS.
2. CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY FOR ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR THE ENGINEER.
3. THE EXISTING UNDERGROUND UTILITIES SHOWN WERE PLOTTED FROM EXISTING AVAILABLE RECORDS OBTAINED FROM THE UTILITY COMPANIES. NEITHER THE OWNER NOR THE ENGINEER SHALL BE RESPONSIBLE FOR THE ACCURACY OF SUCH RECORDS AND DO NOT GUARANTEE THAT ALL UNDERGROUND FACILITIES ARE LOCATED PRECISELY AS SHOWN. PRIOR TO BEGINNING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN FROM ALL UTILITY COMPANIES THE FIELD LOCATION OF THEIR UNDERGROUND FACILITIES. ALL KNOWN OWNERS OF UNDERGROUND FACILITIES IN THE AREA CONCERNED SHALL BE ADVISED OF PROPOSED WORK AT LEAST 72 HOURS PRIOR TO THE START OF ACTUAL EXCAVATION.
4. EXCAVATION SAFETY REQUIREMENTS INCLUDE SUBMITTING A CAL OSHA PERMIT OR LETTER OF NOTIFICATION TO THE CITY BEFORE EXCAVATION OF OVER FIVE FEET DEEP. REFER TO CITY CONSTRUCTION SPECIFICATIONS SEC. 8.19.04 "EXCAVATION SAFETY" FOR ADDITIONAL REQUIREMENTS.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING FROM DAMAGE ALL EXISTING IMPROVEMENTS THAT ARE TO REMAIN. SUCH IMPROVEMENTS THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT HIS EXPENSE.
6. PRIOR TO COMMENCING ANY WORK, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE EACH UTILITY COMPANY LOCATE IN THE FIELD THEIR MAIN AND SERVICE LINES. THE CONTRACTOR SHALL NOTIFY MEMBERS OF THE UNDERGROUND SERVICE ALERT (U.S.A.) 48 HOURS IN ADVANCE OF ANY EXCAVATION WORK BY CALLING 800-227-2600.
7. PRIOR TO SUBMITTING A BID, THE CONTRACTOR SHALL PERFORM THE NECESSARY EARTHWORK CALCULATIONS TO DETERMINE THE AMOUNT OF CUT AND FILL AND RESULTANT EXCESS OR SHORTAGE OF DIRT ONSITE, AND NOTIFY THE CIVIL ENGINEER OF HIS RESULTS. THIS MAY NOT BE A BALANCED SITE.
8. APPROPRIATE DUST CONTROL IS TO BE PROVIDED FOR THE ALLEVIATION OR PREVENTION OF A DUST NUISANCE AT THE CONTRACTOR'S EXPENSE. THE PORT OF STOCKTON PROVIDES CONSTRUCTION WATER AT NO COST TO THE CONTRACTOR.
9. ALL CONSTRUCTION STAKING SHALL BE DONE BY DILLON & MURPHY. STAKING BY OTHERS SHALL BE AT THEIR OWN RISK AND DILLON & MURPHY IS NOT RESPONSIBLE FOR FIELD CHANGES.
10. CONTRACTOR SHALL GIVE THE CIVIL ENGINEER A MINIMUM OF 48 HOURS NOTIFICATION PRIOR TO ANY AND ALL STAKING.
11. IT IS THE INTENT OF THE PORT OF STOCKTON TO HAVE THE SUCCESSFUL BIDDER PREPARE THEIR BID ANTICIPATING THAT THEY WILL BE PROVIDING A COMPLETE JOB AS SHOWN ON THE PLANS AND DESCRIBED IN THE SPECIFICATIONS.
12. DILLON & MURPHY, CONSULTING CIVIL ENGINEERS WILL NOT PROVIDE ELECTRONIC FILES OR VERTICAL AND HORIZONTAL CONTROL POINTS TO BE USED IN PREPARATION OF "GPS STAKING MODELS" OR FOR USE IN "GPS CONSTRUCTION STAKING" ACTIVITIES TO ANY INDIVIDUAL OR COMPANY WHO IS NOT LICENSED IN THE STATE OF CALIFORNIA TO PRACTICE CIVIL ENGINEERING AND/OR PROFESSIONAL LAND SURVEYING.
13. ALL FIRE HYDRANTS THAT ARE AFFECTED AS PART OF THE IMPROVEMENTS SHALL HAVE BLUE REFLECTIVE FIRE HYDRANT MARKERS PLACED ON THE STREET PAVEMENT ADJACENT TO THE FIRE HYDRANT LOCATION.
14. ALL MANHOLE RIMS, DROP INLET GRATES, SANITARY SEWER CLEANOUTS, WATER VALVE BOXES, SURVEY MONUMENT BOXES AND MISCELLANEOUS UTILITY BOXES SHALL BE ADJUSTED TO FINISH GRADE TO MATCH THE CROSS SLOPE.
15. ALL STRIPING AND MARKINGS SHALL CONFORM TO CALTRANS STANDARD SPECIFICATIONS SECTION 84 "TRAFFIC STRIPES AND PAVEMENT MARKINGS" AND SHALL BE THERMOPLASTIC.
16. TRAFFIC CONTROL SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION PER THE PROJECT SPECIFICATIONS.
17. A PORTION OF THIS PROJECT IS IN A TWC AREA. CONTRACTORS AND SUB-CONTRACTORS SHOULD CONSIDER COST FOR TWC CARDS IN THEIR FINAL BID.
18. BENCHMARK: PT. NO. 34 ELEV. 7.88
DESCRIPTION: CHISLED "+" ON TOP OF FH 119 APPROXIMATELY 100' N.E. OF OLD HOTEL.



VICINITY MAP WEST COMPLEX

N.T.S.



VICINITY MAP

N.T.S.

LEGEND

GM	GAS METER	FP	FIRE PROTECTION
FW	FIRE WATER	FV	FIRE VALVE
PW	POTABLE WATER	MW	MONITORING WELL
ELECT.	ELECTRICAL	DRWY	DRIVEWAY
SSCO	SANITARY SEWER CLEAN OUT	FH	FIRE HYDRANT
CO	CLEAN OUT	AC	ASPHALT CONCRETE
TYP	TYPICAL	FD	FOUND
CONC	CONCRETE	FA	FIRE ALARM
BLDG	BUILDING	GP	GATE POST
CB	CATCH BASIN	E/P	EDGE OF PAVEMENT
PP	POWER POLE	EX	EXISTING
VC	VALVE COVER	MAN.	MANUAL
SDMH	STORM DRAIN MANHOLE	W	WATER
SSMH	SANITARY SEWER MANHOLE	OHW	OVER HEAD WIRE
ETW	EDGE TRAVELED WAY	LF	LINEAR FOOT
MH	MANHOLE	CL	CENTERLINE
***	BOLLARDS	⊗	LIGHT
→	GUY WIRE	+++++	TRACKS
⊗	VALVE	⊗	VEGETATION
⊗	POWER POLE	---	PROPOSED SAWCUT
⊗	GATE CONTROL BOX	⊗	FIRE HYDRANT
⊗	SIGN	⊗	MONITORING WELL
⊗	FENCE	⊗	TREE
⊗	BENCHMARK	■	PROPOSED PAVING
⊗	CATCH BASIN	□	EX. PAVING
⊗	TREE		
⊗	LIGHT POLE		
⊗	CONTROL		

BENCHMARK INFORMATION:

WEST COMPLEX:
BENCH MARK: #34 EL: 7.88
DESCRIPTION: CHISLED "+" ON TOP OF FH 119
APPROXIMATELY 100' N.E. OF OLD HOTEL.
DATUM: NGVD 29

OWNER:

PORT OF STOCKTON
2201 W. WASHINGTON STREET
STOCKTON, CA 95203
(209) 946-0246



DILLON & MURPHY CONSULTING CIVIL ENGINEERS

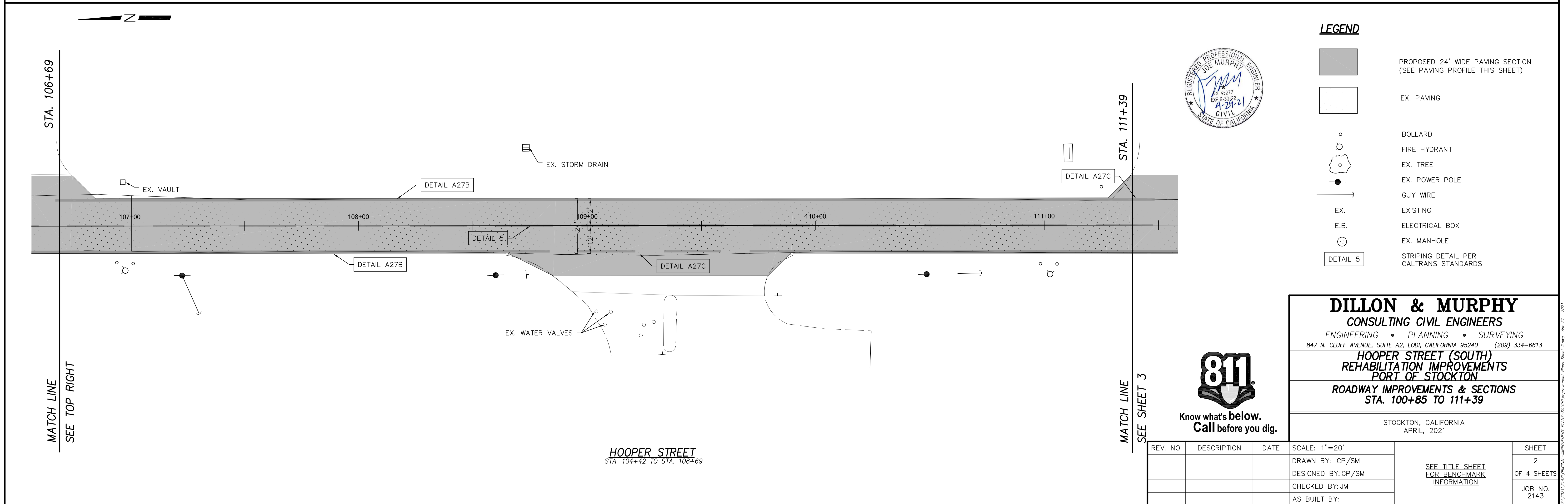
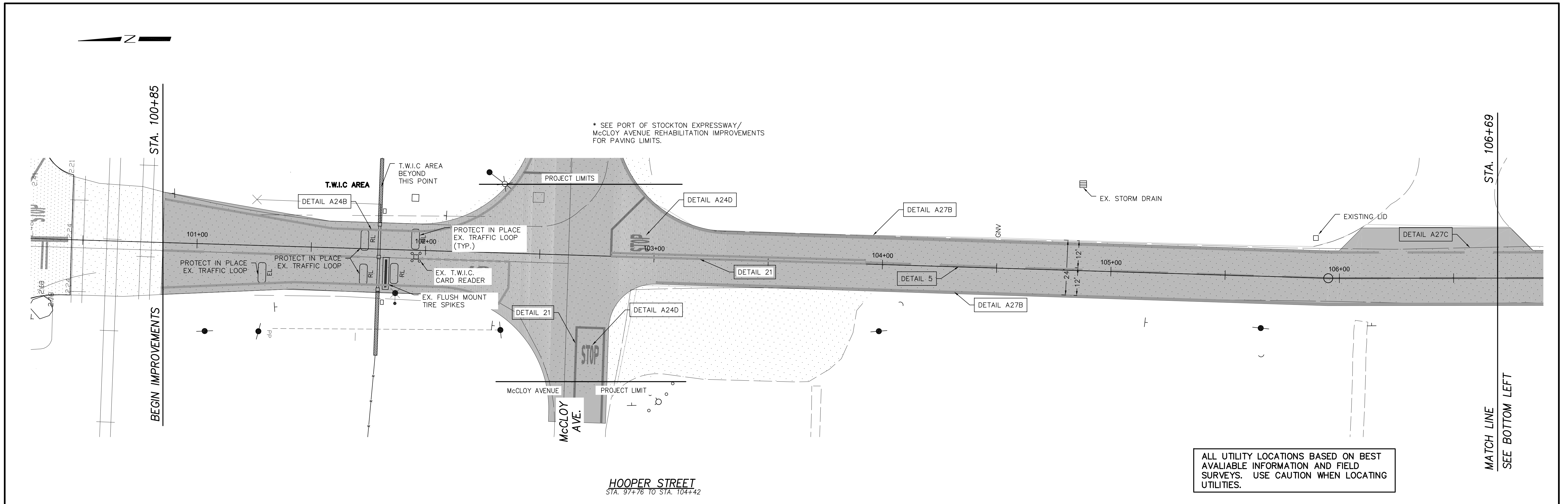
ENGINEERING • PLANNING • SURVEYING
847 N. CLUFF AVENUE, SUITE A2, LODI, CALIFORNIA 95240 (209) 334-6613

HOOPER STREET (SOUTH) REHABILITATION IMPROVEMENTS PORT OF STOCKTON

TITLE SHEET

STOCKTON, CALIFORNIA
APRIL, 2021

REV. NO.	DESCRIPTION	DATE	SCALE: NONE	SEE TITLE SHEET FOR BENCHMARK INFORMATION	SHEET
			DRAWN BY: CP/SM		1
			DESIGNED BY: CP/SM		OF 4 SHEETS
			CHECKED BY: JM		JOB NO.
			AS BUILT BY:		2143



LEGEND

- PROPOSED 24' WIDE PAVING SECTION (SEE PAVING PROFILE THIS SHEET)
- EX. PAVING
- BOLLARD
- FIRE HYDRANT
- EX. TREE
- EX. POWER POLE
- GUY WIRE
- EX.
- E.B. ELECTRICAL BOX
- EX. MANHOLE
- STRIPING DETAIL PER CALTRANS STANDARDS



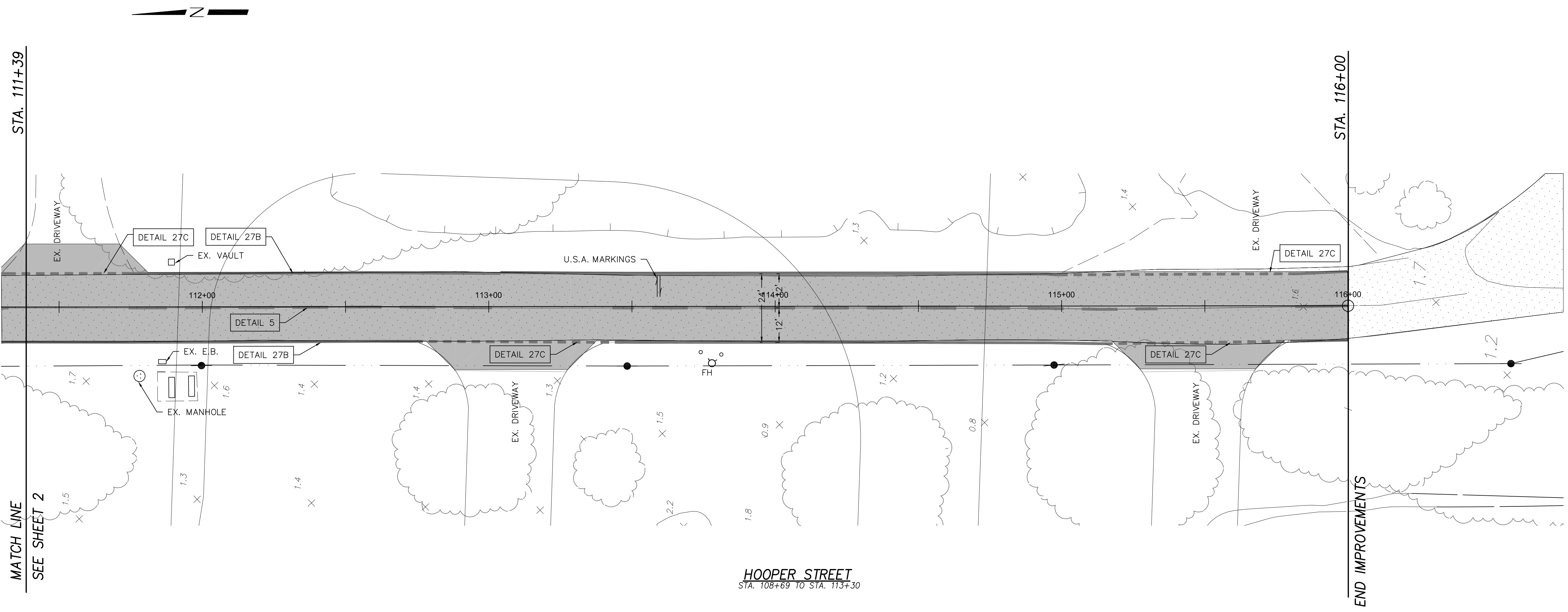
Know what's below.
Call before you dig.


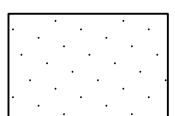









DILLON & MURPHY
CONSULTING CIVIL ENGINEERS
ENGINEERING • PLANNING • SURVEYING
847 N. CLIFF AVENUE, SUITE A2, LODI, CALIFORNIA 95240 (209) 334-6613

**HOOPER STREET (SOUTH)
REHABILITATION IMPROVEMENTS
PORT OF STOCKTON
ROADWAY IMPROVEMENTS & SECTIONS
STA. 100+85 TO 111+39**

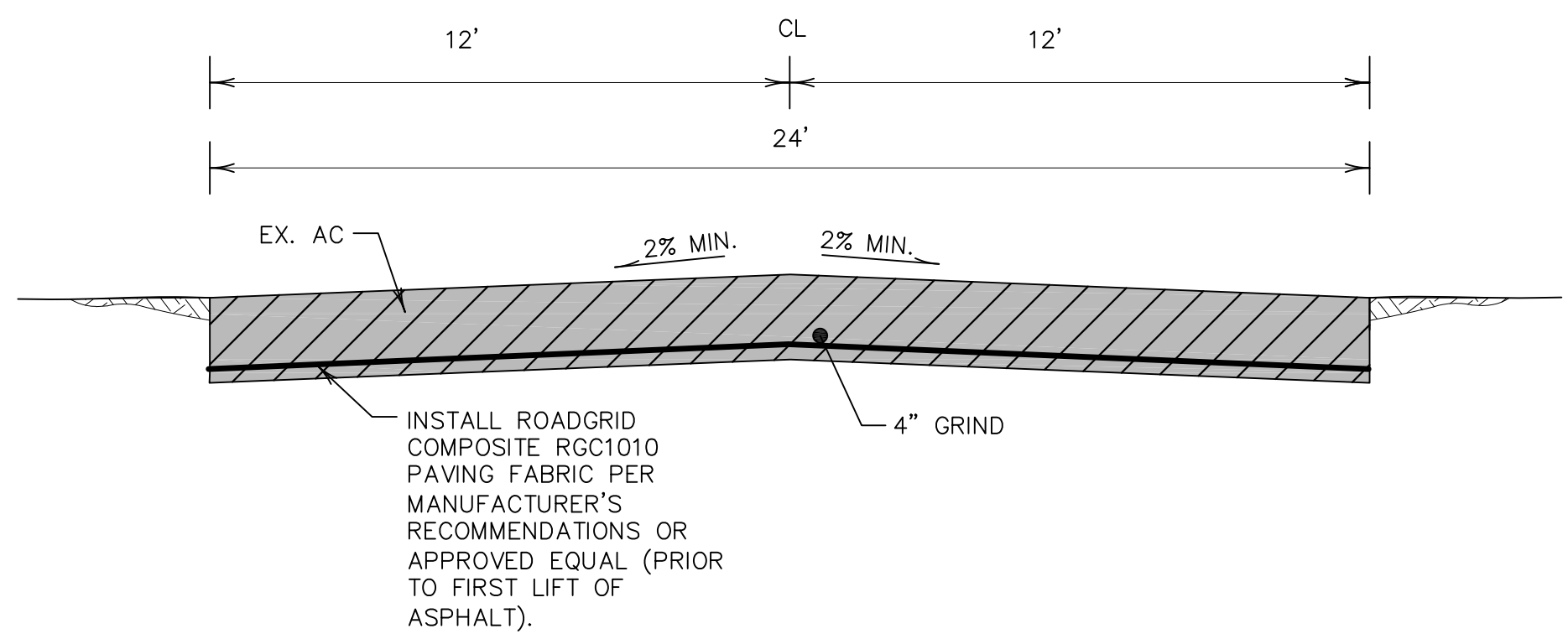
STOCKTON, CALIFORNIA APRIL, 2021		SEE TITLE SHEET FOR BENCHMARK INFORMATION		SHEET 2 OF 4 SHEETS JOB NO. 2143
-------------------------------------	--	---	--	--

REV. NO.	DESCRIPTION	DATE	SCALE: 1"=20'



- LEGEND**
-  PROPOSED 24' WIDE PAVING SECTION (SEE PAVING PROFILE THIS SHEET)
 -  EX. PAVING
 -  BOLLARD
 -  FIRE HYDRANT
 -  EX. TREE
 -  EX. POWER POLE
 -  GUY WIRE
 -  EX. EXISTING
 -  ELECTRICAL BOX
 -  EX. MANHOLE
 -  STRIPING DETAIL PER CALTRANS STANDARDS

ALL UTILITY LOCATIONS BASED ON BEST AVAILABLE INFORMATION AND FIELD SURVEYS. USE CAUTION WHEN LOCATING UTILITIES.



GRIND & OVERLAY PROFILE
NOT TO SCALE



DILLON & MURPHY
CONSULTING CIVIL ENGINEERS
ENGINEERING • PLANNING • SURVEYING
847 N. CLUFF AVENUE, SUITE A2, LODI, CALIFORNIA 95240 (209) 334-6613
HOOPER STREET (SOUTH)
REHABILITATION IMPROVEMENTS
PORT OF STOCKTON
ROADWAY IMPROVEMENTS
STA. 111+39 TO 116+00

STOCKTON, CALIFORNIA
APRIL, 2021

REV. NO.	DESCRIPTION	DATE	SCALE: 1" = 20'	SHEET
			DRAWN BY: CP/SM	3
			DESIGNED BY: CP/SM	OF 4 SHEETS
			CHECKED BY: JM	JOB NO. 2143
			AS BUILT BY:	

SEE TITLE SHEET FOR BENCHMARK INFORMATION

S:\2021\12143\ROADWAY IMPROVEMENTS\PLANS\300701\Improvements - Hooper Street 3.dwg Apr 27, 2021

EROSION CONTROL NOTES:

1. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED DURING CONSTRUCTION BY THE CONTRACTOR PER THE EROSION AND SEDIMENT CONTROL STANDARDS AND SPECIFICATIONS LATEST REVISION.
2. ALL DRAIN INLETS WHICH COULD RECEIVE DRAINAGE IN UNPAVED AND PAVED AREAS DURING THE WET SEASON (OCTOBER 1 TO APRIL 15) SHALL BE SEALED WITH VISQUEEN MEMBRANE.
3. VISQUEEN MEMBRANE SHALL BE REMOVED BY THE CONTRACTOR UPON ACCEPTANCE OF DEVELOPMENT IMPROVEMENTS.
4. CONTRACTOR TO REMOVE ANY SEDIMENTATION DEPOSITED ON EXISTING PAVED ROADWAYS PRIOR TO LEAVING THE SITE, IF POSSIBLE, AND IN ALL CASES WITHIN 24 HOURS.
5. CONTRACTOR TO MAINTAIN STREET FRONTAGE SWALES.
6. INSTALL 9" DIAMETER STRAW WATTLES ALONG PERIMETER OF PROJECT AS SHOWN ON THESE PLANS.
7. CONTRACTOR TO INSPECT SITE AFTER FIRST SIGNIFICANT RAINFALL AND MAINTAIN "STRAW WATTLES", WHERE NECESSARY AND REMOVE ACCUMULATED SILT AT LOCATIONS WHERE EROSION HAS OCCURRED AND SEDIMENT HAS GATHERED.
8. ALL IMPLEMENTED MEASURES SHALL BE MAINTAINED WEEKLY (OR SOONER, DEPENDING ON NEED).
9. STORM WATER POLLUTION PREVENTION PLAN (SWPPP) TO BE KEPT ON SITE AT ALL TIME.
10. BEST MANAGEMENT PRACTICES (BMP'S) SHOWN HERE ARE MINIMUM REQUIREMENTS FOR SATISFACTION OF THE NPDES SWPPP MANAGEMENT PLAN.
11. CONTRACTOR TO GRADE AREA FOR WORKER'S VEHICLES. WATER AT LEAST DAILY TO RETARD DUST OR TAKE MORE EXTENSIVE MEASURES AS NECESSARY.
12. TRASH COLLECTION AREA SHALL BE INSTALLED AND FENCED AS NECESSARY TO STORE ALL DEBRIS AND SECURE THE DEBRIS FROM BEING MOVED BY WIND.
13. PORTABLE RESTROOMS SHALL NOT BE LOCATED CLOSER THAN 50' FROM CATCH BASINS.

CLEARING AND GRADING NOTES:

1. GRADING OPERATIONS SHALL COMPLY WITH THE DUST CONTROL REQUIREMENTS.
2. CONTRACTOR SHALL REMOVE ANY IRRIGATION LINES ENCOUNTERED.

STORM WATER POLLUTION PREVENTION PLAN

WITH THE EXCEPTION OF PROPERLY DESILTED WATER FROM SITE DEWATERING, THE CONTRACTOR SHALL NOT DISCHARGE ANY MATERIALS AN/OR LIQUIDS TO THE STORM DRAINAGE SYSTEM. ACTIVITIES OF PARTICULAR CONCERN ARE:

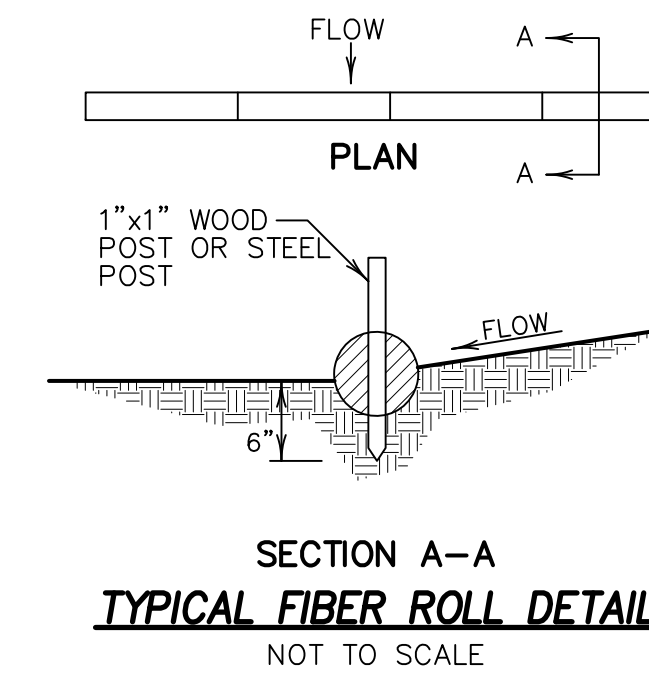
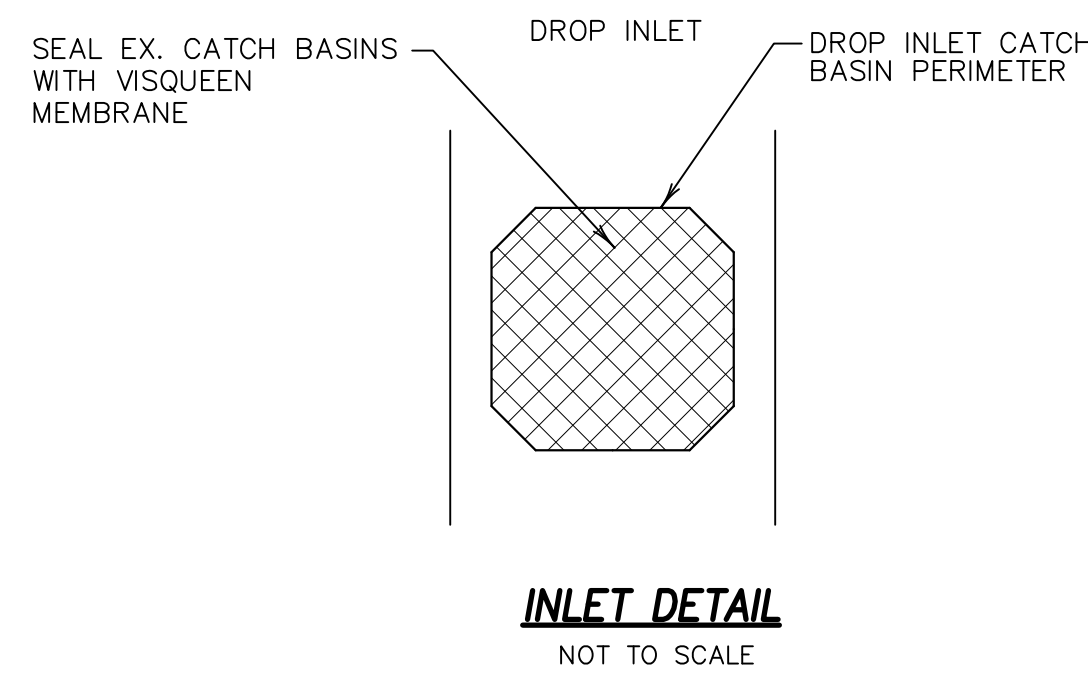
- A. CONTRACTOR SHALL PROVIDE A CONCRETE TRUCK CLEAN-OUT AREA. PORT OF STOCKTON INSPECTOR APPROVED BEST MANAGEMENT PRACTICES SHALL AT ALL TIMES BE FULLY IMPLEMENTED AT AND AROUND THE CLEAN-OUT AREA.
- B. TACK COAT AND PRIME COAT ASPHALTS SHALL BE CAREFULLY SPRAYED AND ANY EXCESS MATERIAL SPILLED SHALL BE CLEANED UP IMMEDIATELY BY PROPER METHODS.
- C. ALL EQUIPMENT REFUELING IN THE PROJECT AREA SHALL BE CAREFULLY DONE TO AVOID SPILLAGE. ANY SPILLS SHALL BE CONTAINED AND CLEANED UP IMMEDIATELY IN ACCORDANCE WITH STATE AND LOCAL REQUIREMENTS. ALL FUELING VEHICLES SHALL BE EQUIPPED WITH SPILL CLEANUP MATERIALS AND EQUIPMENT.
- D. CONTRACTOR SHALL CONTROL AND PROPERLY DISPOSE OF ALL LIQUIDS DURING SAWCUTTING ACTIVITIES.

GENERAL

1. A CONSTRUCTION ENTRANCE SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF GRADING. LOCATION OF THE ENTRANCE MAY BE ADJUSTED BY THE CONTRACTOR TO FACILITATE GRADING OPERATIONS. ALL CONSTRUCTION TRAFFIC ENTERING OR EXITING THE PAVED ROAD MUST CROSS THE CONSTRUCTION ENTRANCE.
2. CHANGES TO THIS EROSION CONTROL PLAN TO MEET FIELD CONDITIONS SHALL BE MADE ONLY WITH APPROVAL OF, OR AT THE DIRECTION OF THE PROJECT ENGINEER WITH THE APPROVAL OF THE PORT OF STOCKTON. DURING THE RAINY SEASON, ROADWAYS SHALL BE KEPT CLEAR OF EARTH MATERIAL AND DEBRIS. THE SITE SHALL BE MAINTAINED SO THAT A MINIMUM OF SEDIMENT LADEN RUNOFF ENTERS THE STORM DRAINAGE SYSTEM AND ROADWAYS.
3. CONTROL MEASURES SHALL BE INSPECTED AND REPAIRED AT THE END OF EACH WORKING DAY AND, IN ADDITION, AFTER EACH STORM.

DUST CONTROL NOTES:

1. DUST CONTROL IS THE CONTRACTOR'S SOLE RESPONSIBILITY WHEN WORKING ON-SITE. THE WORK SHOWN ON THIS SHEET SHALL BE INCLUDED IN THE BID. THE CONTRACTOR SHALL ALSO WATER THE SITE AS AN EXTRA, WHEN REQUESTED BY THE PORT OF STOCKTON.
2. ALL MOTORS SHALL BE KEPT PROPERLY TUNED AND MAINTAINED.
3. EARTH MOVING ACTIVITIES SHALL CEASE IF WIND EXCEEDS 20 MPH AVERAGE OVER ONE HOUR.
4. ON-SITE VEHICLE SPEEDS SHALL BE LIMITED TO 15 MPH.
5. ALL OPERATIONS SHALL COMPLY WITH THE REQUIREMENTS OF THE SAN JOAQUIN COUNTY UNIFIED AIR POLLUTION CONTROL DISTRICT.
6. ALL AREAS WITH VEHICLE TRAFFIC SHOULD BE WATERED PERIODICALLY FOR STABILIZATION OF DUST EMISSIONS.
7. STREETS ADJACENT TO THE PROJECT SITE SHOULD BE SWEEP AS NEEDED TO REMOVE SILT WHICH MAY HAVE ACCUMULATED FROM CONSTRUCTION ACTIVITIES.
8. THE AREA DISTURBED BY EARTH MOVING OR EXCAVATION ACTIVITIES SHOULD BE MINIMIZED AT ALL TIMES.
9. ALL MATERIAL TRANSPORTED ON OR OFF-SITE SHALL EITHER BE SUFFICIENTLY WATERED OR SECURELY COVERED TO PREVENT EXCESSIVE AMOUNTS OF DUST.
10. WHEN CONDITIONS ARE DRY, ALL MATERIAL EXCAVATED OR GRADED SHOULD BE SUFFICIENTLY WATERED TO PREVENT EXCESSIVE AMOUNTS OF DUST. WATERING SHOULD OCCUR AT LEAST TWICE A DAY WITH COMPLETE COVERAGE, PREFERABLY IN THE LATE MORNING AND AFTER WORK IS COMPLETED FOR THE DAY.

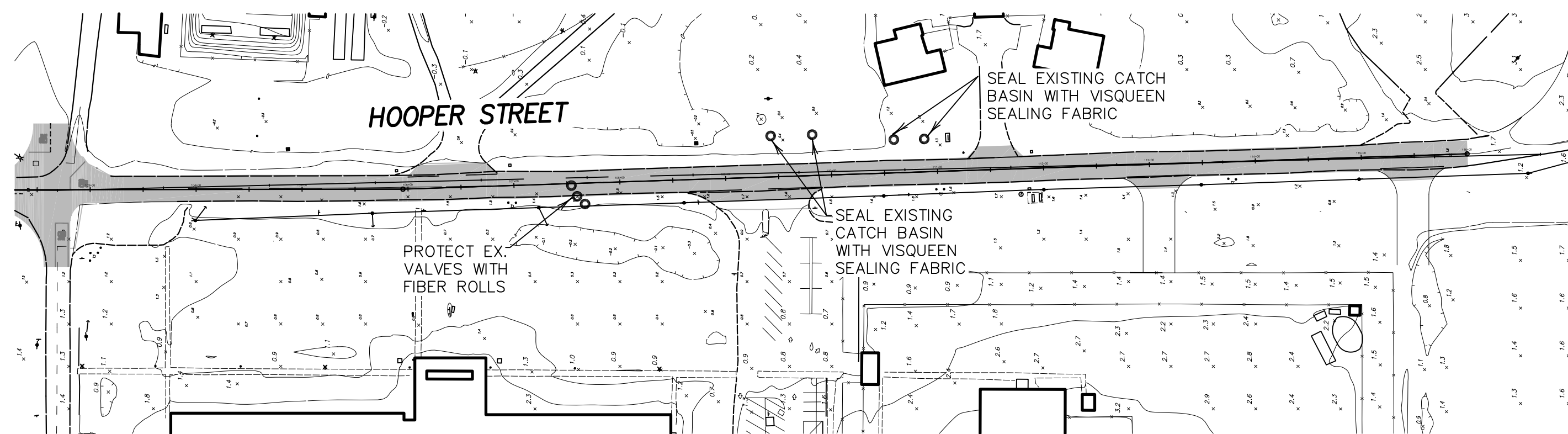


NOTES:

1. DEVELOPER IS RESPONSIBLE FOR REPLACEMENT OF MISSING AND DAMAGED FILTER SCREENS UNTIL THE PROJECT IS ACCEPTED. A ROUTINE MAINTENANCE SCHEDULE IS TO BE MAINTAINED. FILTER SCREENS SHALL BE SECURED TO THE CATCHBASIN DURING THIS PERIOD.
2. COMPRESSED BETWEEN REBAR FRAME AND DICB IS 1" THICK FILTER PAD MATERIAL. FILTER PAD MATERIAL IS DENSE TO RETAIN SILT YET POROUS ENOUGH TO ALLOW WATER DRAINAGE. THE FILTER SYSTEM IS WIRE TIED TO THE DICB IN 2 OR MORE PLACES TO PREVENT SLIPPAGE OR REMOVAL OF UNIT FROM DICB
3. THE FILTER PAD AND GRATE SHALL BE SECURELY ATTACHED TO THE DROP INLET BY WIRE OR TIE-WRAPPS.
4. INLET FILTERS SHALL BE INSPECTED WEEKLY AND AFTER EACH RAINFALL. REPAIRS AND SEDIMENT AND DEBRIS REMOVAL SHALL BE MADE AS NECESSARY
5. INLET FILTERS SHALL BE "CONSTRUCTION" STORM DRAIN FILTERS BY POLLUTION SOLUTION.

NOTES:

1. FILTER ROLLS SHALL BE CONSTRUCTED LONG ENOUGH TO EXTEND ACROSS THE EXPECTED FLOW PATCH.
2. SUPPORT POSTS SHALL BE A MINIMUM 24" LONG 1"x1" WOOD POSTS DRIVEN A MINIMUM OF 6" INTO THE GROUND. POSTS SHALL BE SPACED A MAXIMUM OF 4 FEET APART.
3. FILTER ROLL SHALL BE IMBEDDED A MINIMUM OF 3" INTO EXISTING GROUND.
4. CONTRACTOR SHALL MAKE INSPECTIONS WEEKLY DURING THE WET SEASON, MONTHLY DURING THE DRY SEASON, AND IMMEDIATELY AFTER EACH RAINFALL TO DETERMINE IF REPAIRS AND SEDIMENT REMOVAL IS REQUIRED. SEDIMENT SHALL BE REMOVED BEFORE IT HAS REACHED ONE HALF THE HEIGHT OF THE FILTER ROLL.



SCALE: 1" = 100'



Know what's below.
Call before you dig.



DILLON & MURPHY
CONSULTING CIVIL ENGINEERS
ENGINEERING • PLANNING • SURVEYING
847 N. CLUFF AVENUE, SUITE A2, Lodi, CALIFORNIA 95240 (209) 334-6613
HOOPER STREET (SOUTH)
REHABILITATION IMPROVEMENTS
PORT OF STOCKTON
EROSION CONTROL PLAN
STOCKTON, CALIFORNIA
APRIL, 2021

REV. NO.	DESCRIPTION	DATE	SCALE: 1" = 100'	SEE TITLE SHEET FOR BENCHMARK DETAILS.	SHEET
			DRAWN BY: CP/SM		4
			DESIGNED BY: CP/SM		OF 4 SHEETS
			CHECKED BY:		JOB NO. 2143
			AS BUILT BY:		